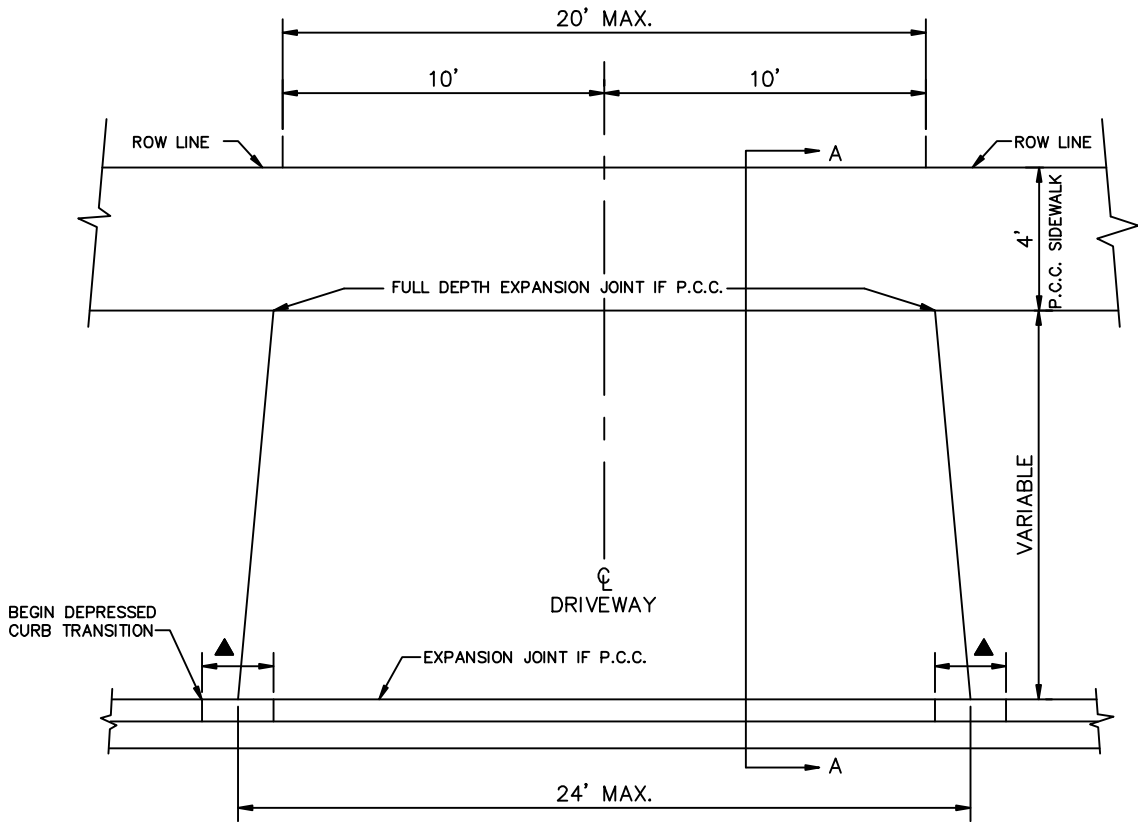


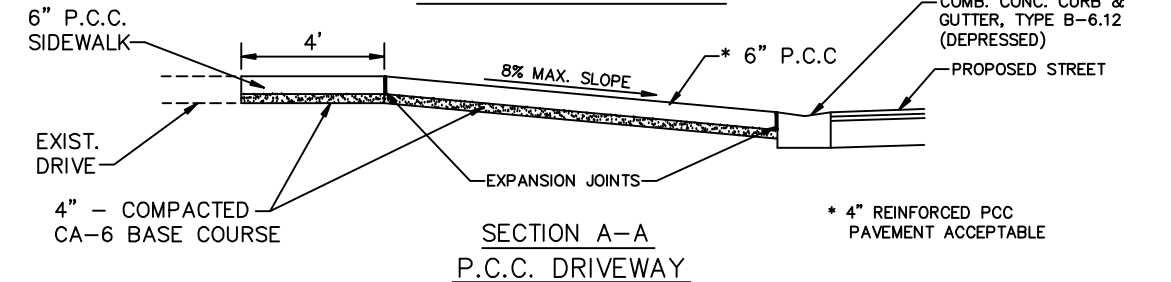
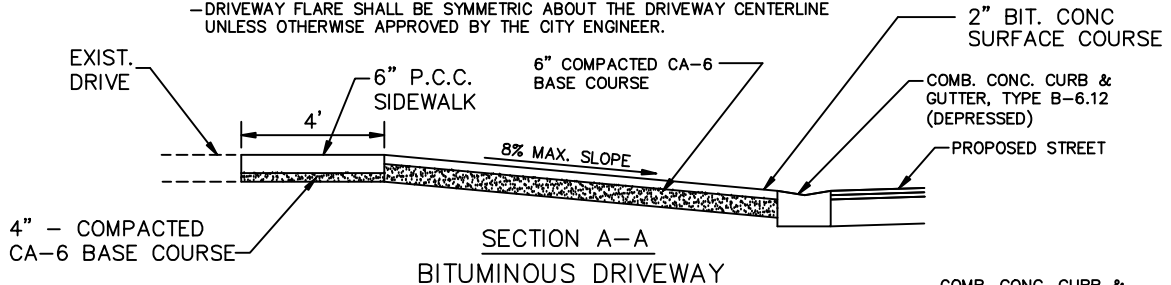
● APPROACH GRADE 8% MAX.

VILLAGE OF LAKE IN THE HILLS	REVISIONS		
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INDUSTRIAL/COMMERCIAL DRIVEWAY DETAIL

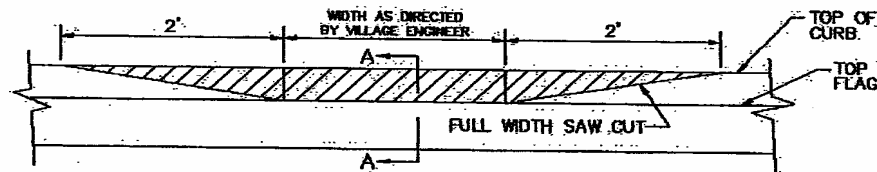


- ▲ - TRANSITION FROM FULL CURB IN 2 LINEAL FEET.
- DRIVEWAY APPROACH SHALL MEET CURB AND GUTTER AT A POINT BETWEEN FULL CURB AND ONE-HALF CURB HEIGHT.
- DRIVEWAY FLARE SHALL BE SYMMETRIC ABOUT THE DRIVEWAY CENTERLINE UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.

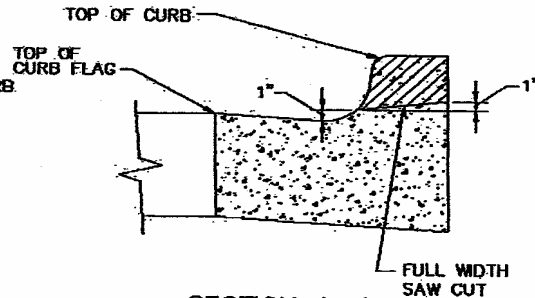


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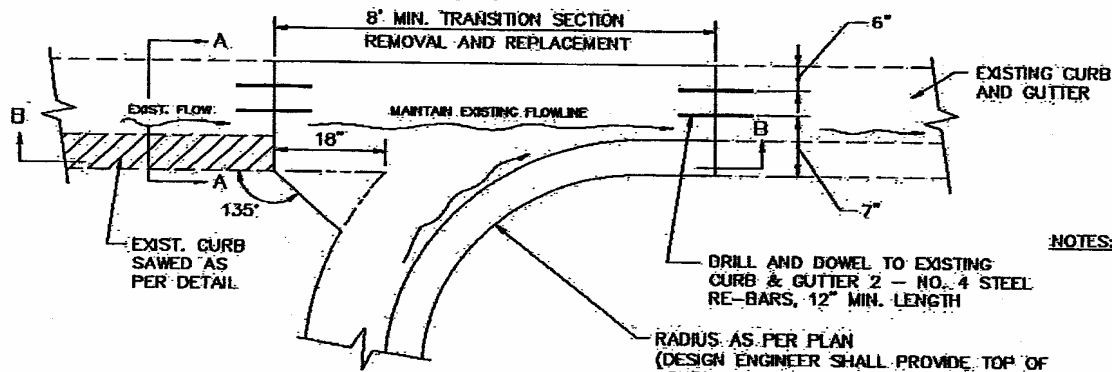
RESIDENTIAL DRIVEWAY  
APPROACH DETAIL



**DRIVEWAY OPENING SECTION  
(SECTION B-B)**



**SECTION A-A**

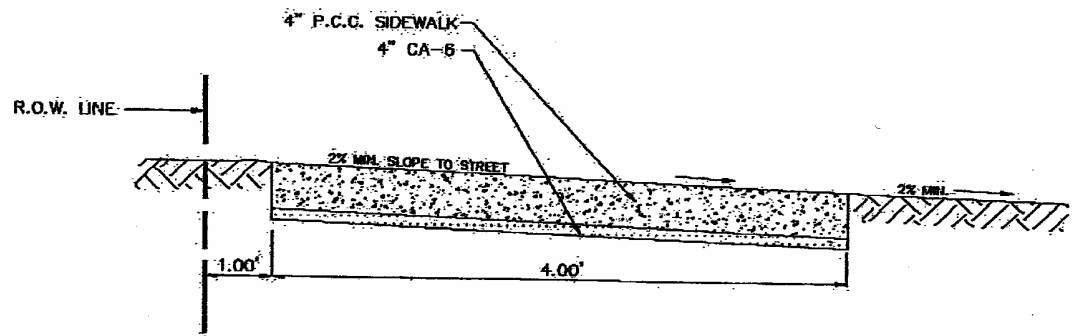


**B 6.12 RETURN**

- NOTES:**
1. PLACE APPROVED SEAL COAT ON AREA THAT HAS BEEN SAWED
  2. APPROVED TRAFFIC CONTROL NECESSARY DURING EXCAVATION/CONSTRUCTION PERIOD.

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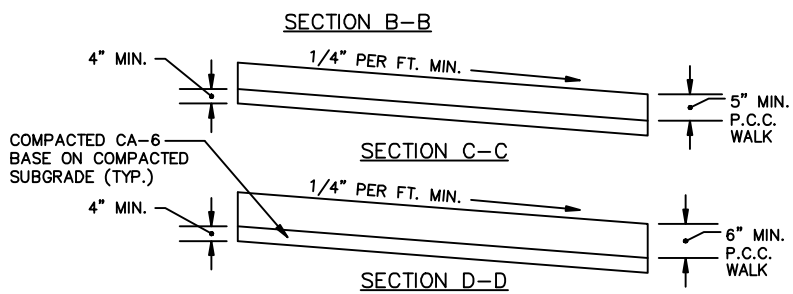
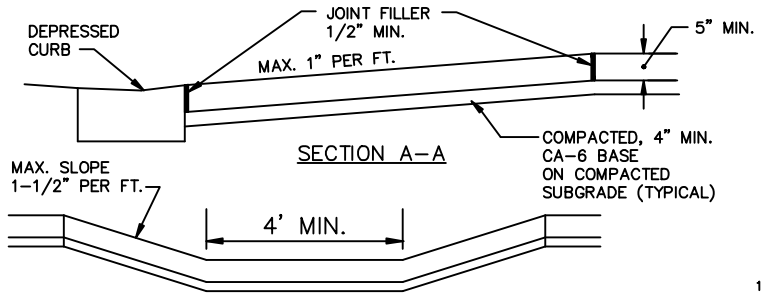
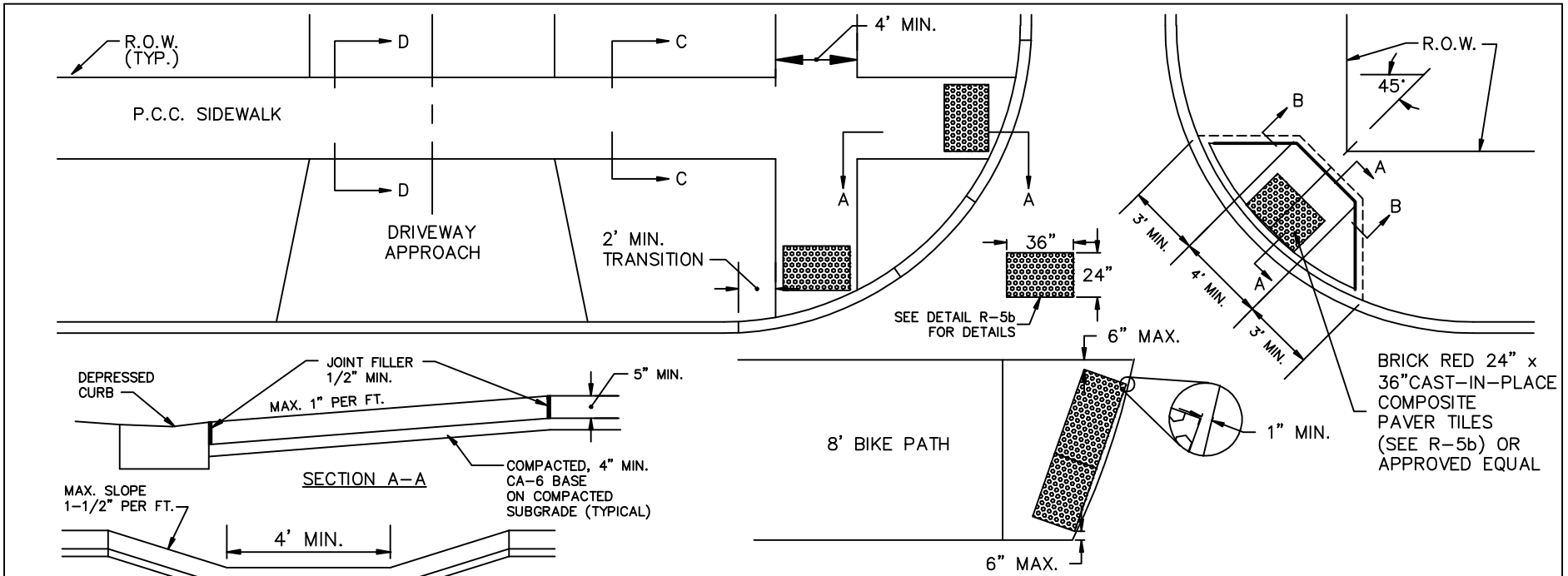
**DEPRESSED CURB ENTRANCE BY SAWING  
(NO POURED-IN-PLACE DEPRESSED CURBS)**



NOTES:  
 SIDEWALK THROUGH DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE VILLAGE OF LAKE IN THE HILLS DETAIL R-1 OR R-2 AS APPROPRIATE. ALL SIDEWALKS SHALL HAVE CONTRACTION JOINTS AT 5' INTERVALS AND EXPANSION JOINTS AT 25' INTERVALS.

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SIDEWALK DETAIL



1. ALL SIDEWALKS SHALL BE CONSTRUCTED OF 6-BAG MIX PORTLAND CEMENT CLASS 'SI' CONCRETE WITH A TRANSVERSE BROOM FINISH AND TOOLED EDGES AND JOINTS IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS OF ROAD AND BRIDGE CONSTRUCTION, SECTION 424. A CURING COMPOUND SHALL BE APPLIED TO ALL SURFACES. SPECIAL ATTENTION SHALL BE GIVEN TO POURING CONCRETE IN COLD WEATHER. NO CONCRETE SHALL BE POURED ON A FROZEN SUBGRADE OR BASE. REFER TO THE VILLAGE'S COLD WEATHER SPECIFICATIONS FOR ALL SUCH CONSTRUCTION.
2. CONCRETE SHALL BE CONSTRUCTED ON A WELL COMPACTED SUBGRADE AND GRANULAR BASE. SUBGRADE, BASE, FORMS, AND ALIGNMENT SHALL BE INSPECTED BY THE VILLAGE ENGINEERING DEPARTMENT PRIOR TO PLACEMENT.
3. CONTRACTION JOINTS SHALL BE PLACED ON 5-0" CENTERS. FULL DEPTH EXPANSION JOINTS ARE REQUIRED EVERY TWENTY FIVE FEET.
4. THE THICKNESS OF SIDEWALKS SHALL BE 5" MIN. ON 4" MIN. CA-6 BASE AND 6" MIN. THICK THRU RESIDENTIAL DRIVEWAY APPROACHES AND 8" THICK ON COMMERCIAL APPROACHES. PEA GRAVEL AND SAND ARE NOT ACCEPTABLE BASE COURSE MATERIAL. THEY SHALL BE A MINIMUM OF 4" WIDE OR CONSISTENT WITH ADJACENT WALKS.
5. THE FINISH GRADE OF THE SIDEWALK SHALL BE ESTABLISHED BY THE PUBLIC WORKS DEPARTMENT WITH A MINIMUM PITCH TOWARD THE STREET OF 1/4" PER FOOT OF WIDTH.
6. NO SIDEWALK MAY BE CONSTRUCTED CLOSER THAN 24" FROM A WATER SERVICE BOX UNLESS APPROVED BY THE PUBLIC WORKS DEPARTMENT.
7. RAMPS SHALL BE LOCATED AS SHOWN ON PLANS WITH CONSIDERATION GIVEN TO ALIGNMENT WITH NORMAL SIDEWALK OR CROSSWALK AND MAINTENANCE OF SUFFICIENT CURB LENGTH AT CORNER RADIUS TO PREVENT VEHICULAR ENCROACHMENT.
8. RAMPS SHALL NOT HAVE A SLOPE GREATER THAN 1" PER FOOT EXCEPT WHERE BUILDING OR OTHER RESTRICTIONS WILL NOT PERMIT.
9. THICKNESS OF RAMPS WILL BE THE SAME AS THE ADJACENT SIDEWALK WITH A MINIMUM 4".

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PUBLIC SIDEWALK AND HANDICAP RAMP DETAIL

**PRODUCT SPECIFICATIONS**  
**CAST-IN-PLACE COMPOSITE PAVER TILES**  
 Inline Dome Pattern: Tactile Warning Surface

**DOME GEOMETRY** *In accordance with ADA Regulations for Detectable Warning on Curb Ramps:* raised truncated domes with a diameter of nominal 0.9", a height of nominal 0.2", and a center-to-center spacing of 1.67" minimum, and 2.35" maximum.

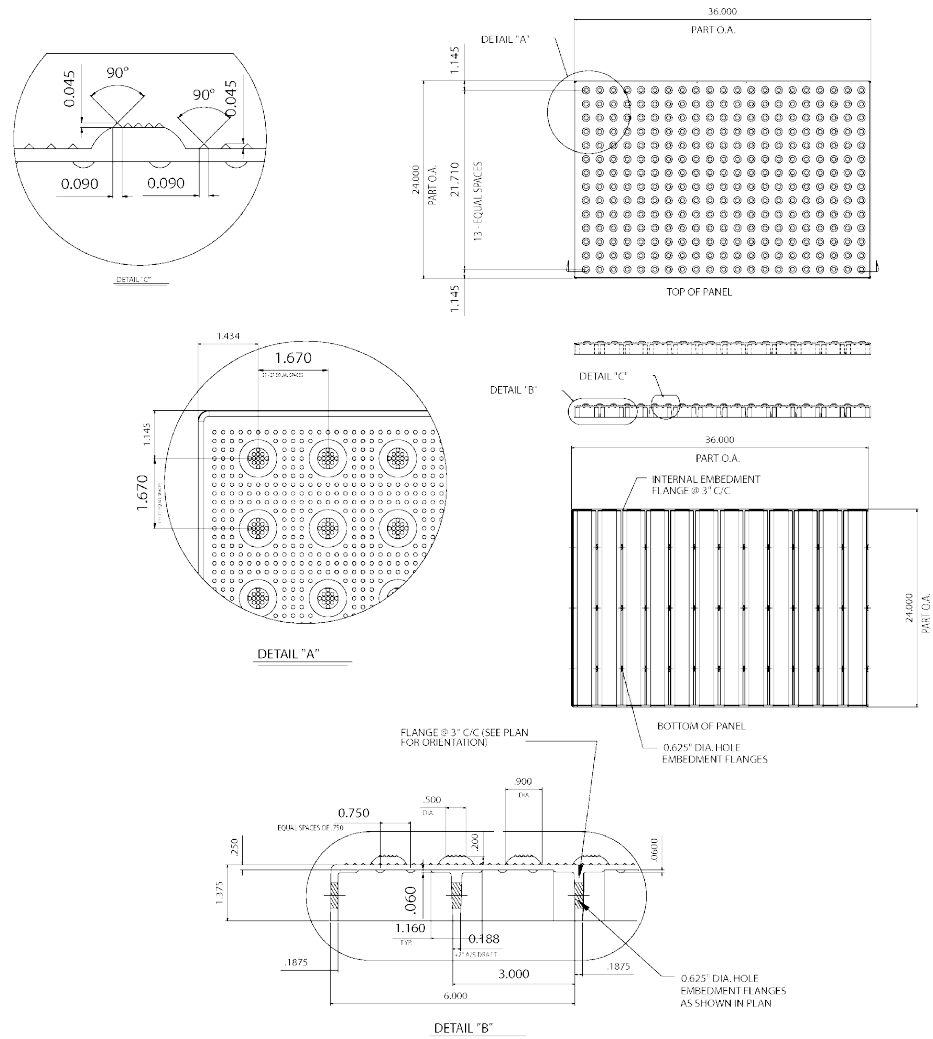
**PANEL DIMENSIONS** Use ADA Solutions Part # 2436IDPave2, #2448IDPave2, #2460IDPave2, Brick Red, or Approved Equal

**MATERIAL** A homogenous glass and carbon reinforced composite which is colorfast and UV stable. Truncated Domes are fiberglass reinforced for enhanced durability. The TWS panel color is uniform throughout and does not rely on any type of paint coating to achieve color stability. Standard colors include: Federal Yellow, Brick Red, Clay Red, Dark Gray, Black, and Blue.

**INSTALLATION** TWS Units are to be used on new curb ramp locations. The TWS Units can be pre-filled with concrete and set in place or pressed into place in the freshly poured concrete.

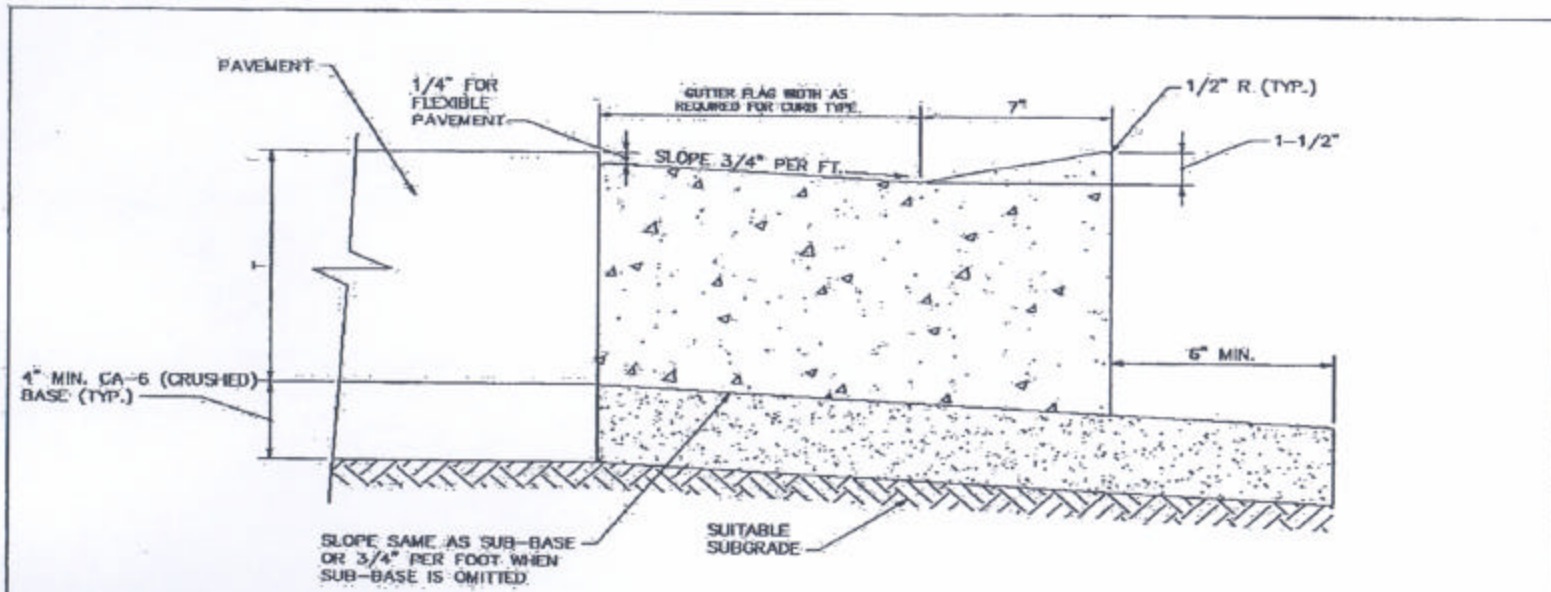
**PHYSICAL CHARACTERISTICS:**

Compressive Strength	28,900 psi	ASTM D 695
Flexural Strength	29,300 psi	ASTM D 790
Water Absorption	.07%	ASTM D 570
Slip Resistance	1.18 Dry/1.05 Wet	ASTM C 1028
Flame Spread Index	20	ASTM E 84
Salt Spray	No Change (200 hours)	ASTM B 117
Chemical Stain Testing	No Deterioration	ASTM 1308
Abrasion Resistance	549	ASTM C 501
Accelerated Weathering	Delta E < 5.0 (2,000 hours)	ASTM G 155
Tensile Strength	11,600 psi	ASTM D 638
Adhesion to Concrete (20° -180°)	No Delamination or Degradation	ASTM C 903
Freeze/Thaw/Heat	No Disintegration	ASTM C 1026



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CAST-IN-PLACE COMPOSITE PAVER TILES  
 BRICK RED, 24" X 36"

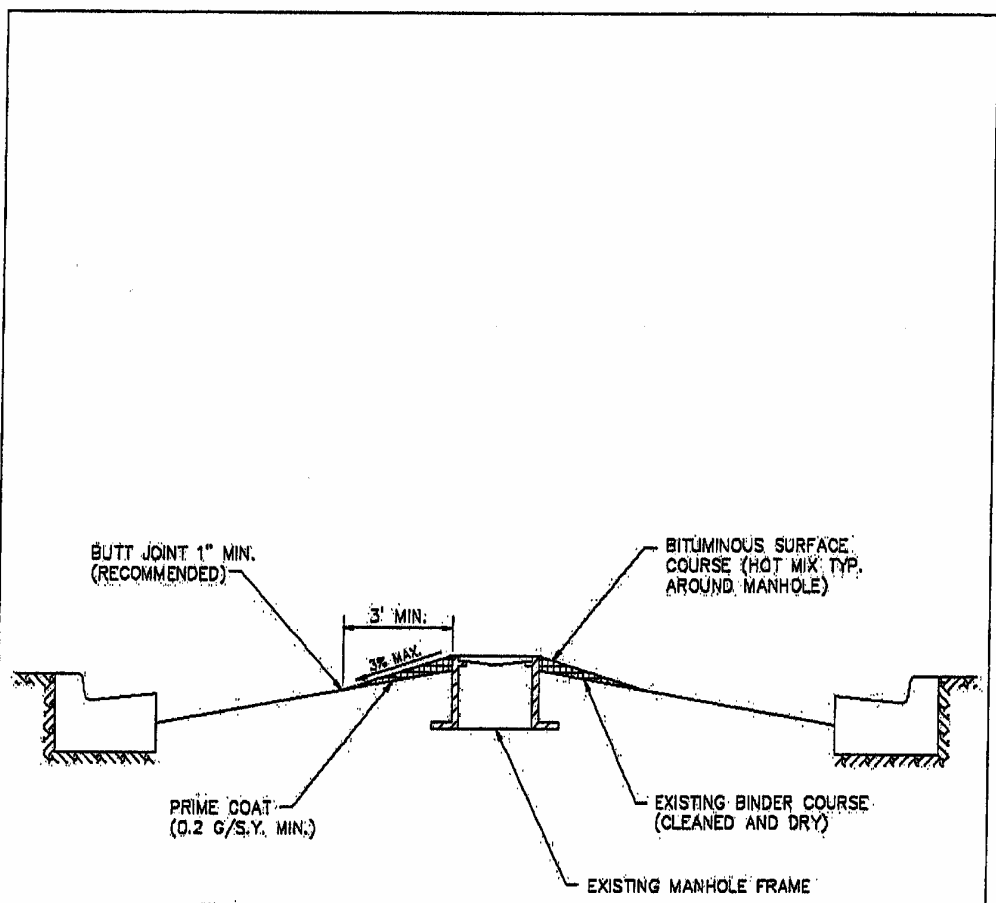


**NOTES:**

1. THE TOP OF CURBS SHALL BE DEPRESSED WHERE THE CURB AND GUTTER IS CONSTRUCTED ACROSS ALLEYS AND FOR PRIVATE DRIVES OR WHERE DIRECTED BY THE ENGINEER OR PUBLIC WORKS DEPARTMENT.
2. THICKNESS OF PAVEMENT "T" WHEN CURB AND GUTTER IS CONSTRUCTED ADJACENT TO FLEXIBLE PAVEMENT, THE VERTICAL THICKNESS OF THE GUTTER SHALL BE 9 INCHES.

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**DEPRESSED CURB FOR INTERSECTIONS**

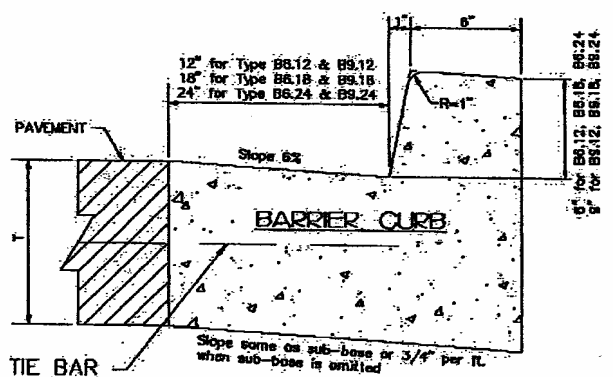


**NOTES:**

1. RAMP TO BE MAINTAINED THROUGHOUT THE WINTER BY DEVELOPER.
2. RAMP MATERIAL TO BE REMOVED BY MILLING OR OTHERWISE REMOVED BEFORE PLACEMENT OF FINAL SURFACE COURSE.

VILLAGE OF LAKE IN THE HILLS PUBLIC WORKS DEPARTMENT APPROVED 00-00-00	REVISIONS NO. BY DATE		
MANHOLE RAMPING DETAIL			R-7





**THICKNESS** - T - Thickness of pavement. When curb & gutter is constructed adjacent to flexible pavement, the vertical thickness of the gutter flag shall be 9". Also, tie bars shall be omitted.

**DRAINAGE OPENINGS** - At all locations where metal casings are to be incorporated in the curb and gutter, a 1" thick preformed joint filler, conforming to the cross sections of the curb and gutter, shall be installed in the curb and gutter a distance of 10 ft. from each side of the metal casing. When the width of the metal casing is less than the width of the curb and gutter, 2 - No. 4 rebars (L = 12" + casing length + 12") shall be incorporated in the continuous portion of concrete gutter in front of the casing.

**TRANSITIONS** - The transition from full height curb to depressed curb shall be made at the rate of 3" per foot of length or flatter.

**JOINTS** - In addition to the requirements of Article 606 of the Standard Specifications, joints shall be constructed as follows.

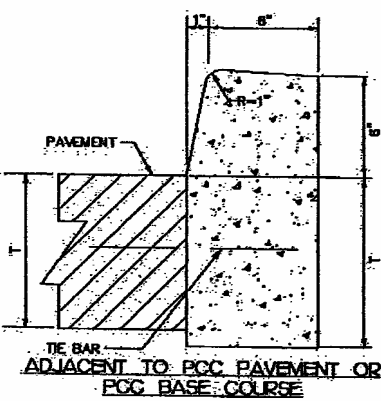
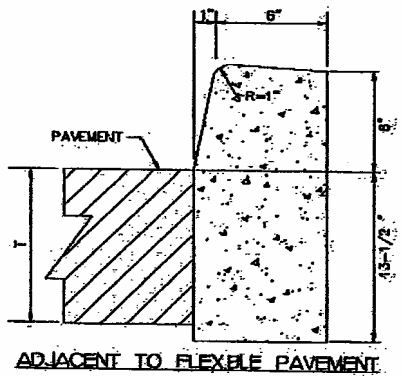
When curb & gutter is constructed adjacent to P.C.C. pavement, contraction joints, spaced at 10' intervals shall be sawcut within 24 hours. Expansion joints shall be installed in the curb and gutter in prolongation with joints in adjacent P.C.C. pavement or base course.

When curb and gutter is constructed adjacent to flexible pavement, a 1" thick preformed expansion joint filler, conforming to the cross-section of the curb and gutter shall be installed every 90' max., at points of curvature for short radius curves and at construction joints. Contraction joints shall be sawcut within 24 hours between expansion joints at distances not to exceed 15 feet.

All expansion joints shall be placed 5 feet on either side of every drainage structure located within the curb and gutter. All expansion joints shall be provided with a 1 1/4 dia. X 18" coated smooth dowel bar conforming to Article 1006.11(b) of the Standard Specifications. The dowel bar shall be fitted with a cap having a pinched stop that will provide 1" of expansion.

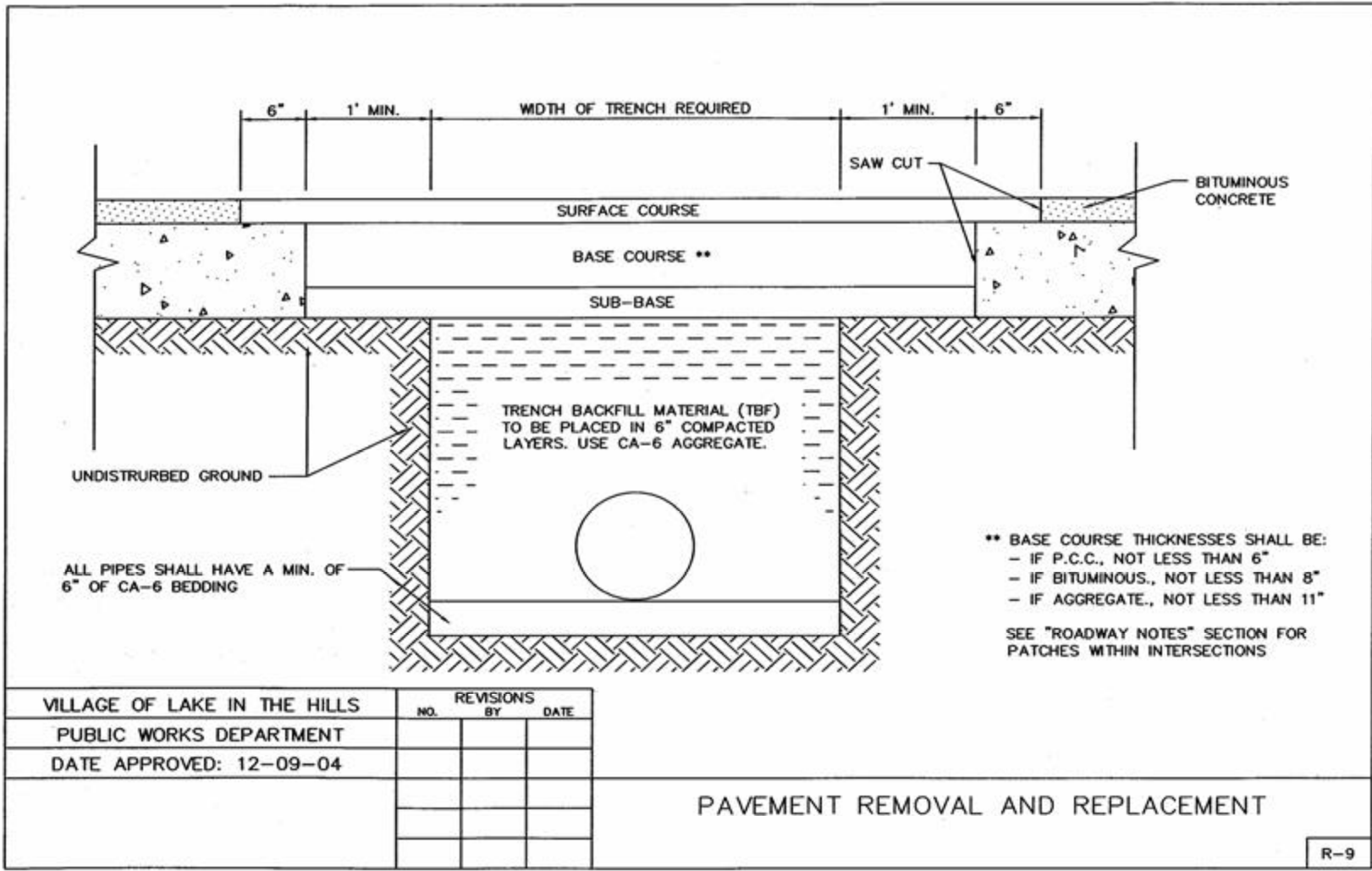
Curb & gutter located over utility crossings and at depressions for sidewalks shall be reinforced with 2-no. 4 bars on 12" centers of a length of 20' as approved by the Engineer.

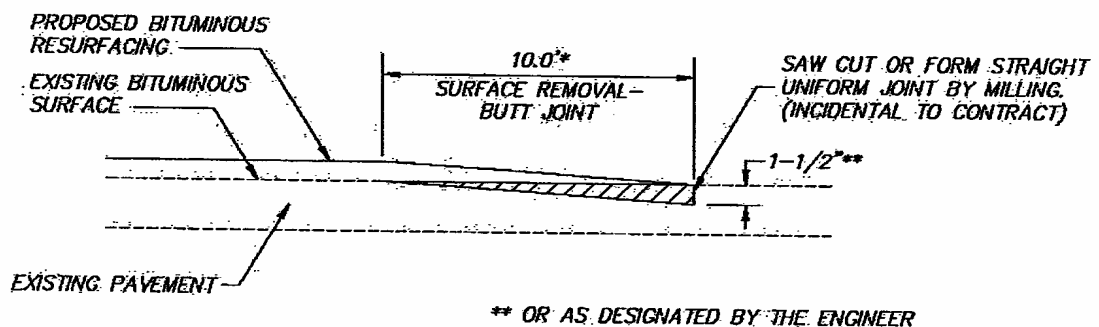
All construction joints shall be provided with 1/2" dia. deformed steel tie bars 30" long conforming to AASHTO M-31 or M-53. Tie bars shall be placed on 9"± centers (minimum 2 per joint).



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## CONCRETE CURB AND GUTTER DETAIL





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APPROVED. 1-19-99			

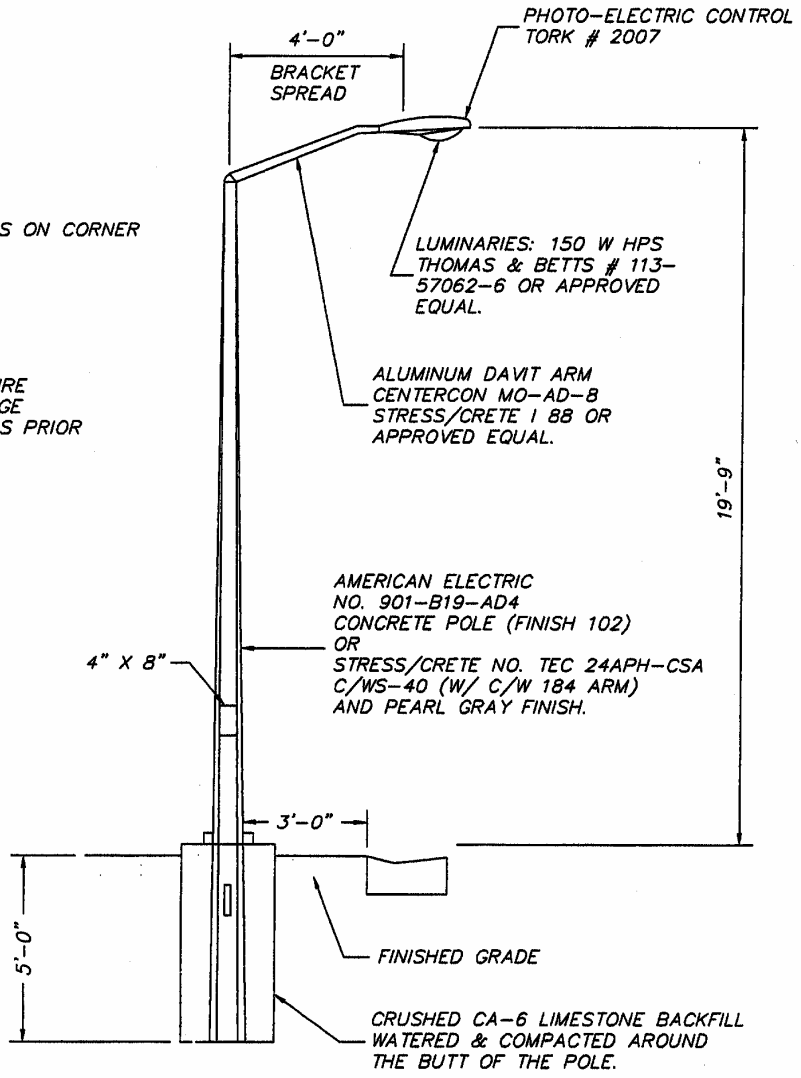
BITUMINOUS SURFACE REMOVAL  
BUTT JOINT DETAIL

**NOTES**

BRACKET MOUNTING IS ON CORNER OF POLE NOT FLATS.

TYPE III OR IV I.E.S.  
 @ INTERSECTIONS  
 TYPE II OTHERWISE.

STREET LIGHTS REQUIRE INSPECTION BY VILLAGE OF LAKE IN THE HILLS PRIOR TO TURN ON.



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RESIDENTIAL STREET LIGHT  
 DETAIL